

Harbourfront Connectivity Study

Connecting "Forgotten" Waterfronts



Harboufront Vision



Quarry Bay Park



Tsim Sha Tsui Promenade

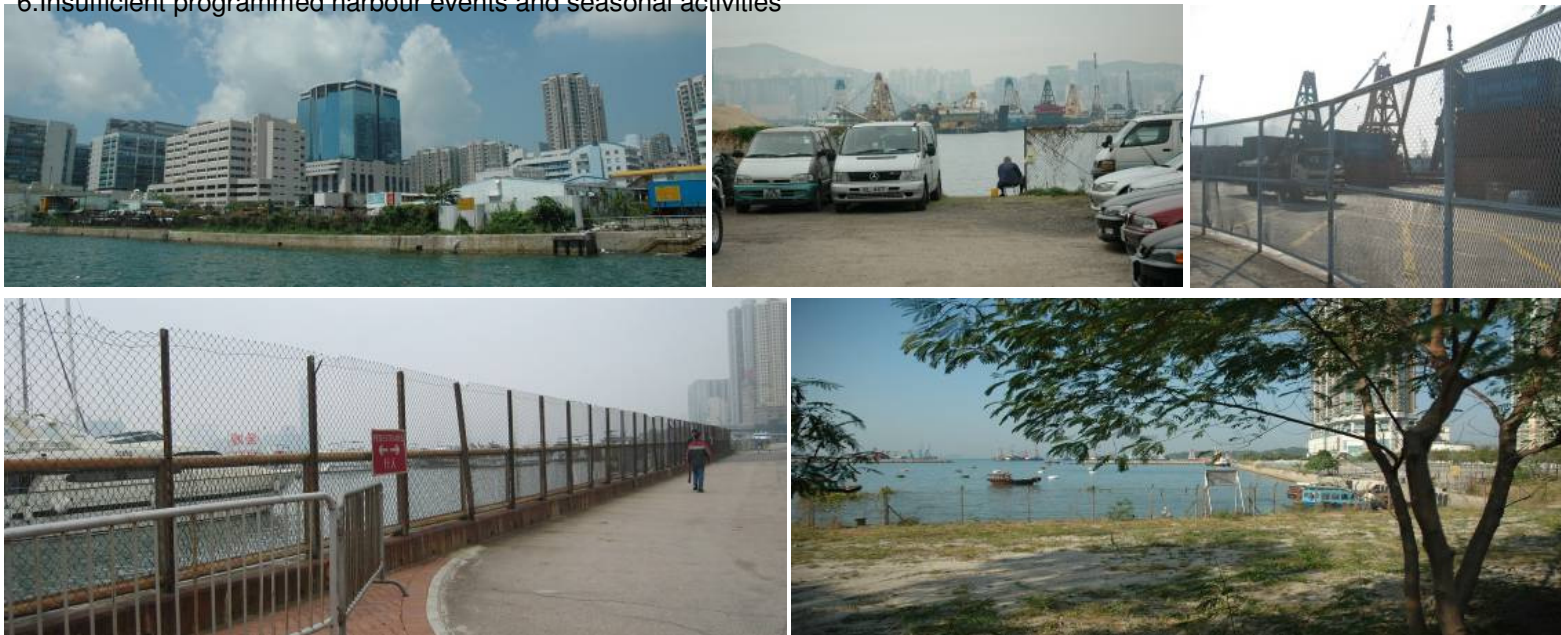
Informal Connectivity



We want to reach our waterfront, we want to swim, fishing.....

Key Connectivity Issues and Opportunities

1. Incompatible uses - temporary car parking and long term GIC uses
2. Large roads and private developments at the waters edge blocking access
3. Underutilization of existing piers, landing steps and breakwaters for water transport
4. Lack of waterfront destination nodes and proper land and marine interface
5. Lack of signage and way finding
6. Insufficient programmed harbour events and seasonal activities



Objectives for Harbourfront Connectivity

1. Improve connectivity in the “forgotten waterfronts” and provide access to temporary open space uses along the waterfront;
2. Create a continuous waterfront connection for both pedestrians, and cyclists and connect existing and new waterfront promenades;
3. Identify potential waterfront destination nodes and opportunities for water transport in the form of harbour hoppers, water taxi and sampan service;



Objectives for Harbourfront Connectivity

4. Better utilize existing piers, landing steps and breakwaters to create opportunities for water transport and boat mooring etc
5. Provide high quality landscaped areas, flexible open space and activity generators along the waterfront promenade for pedestrian amenity and safety;
6. Provide adequate signage and harbourfront maps to improve way finding to and along the harbourfront.

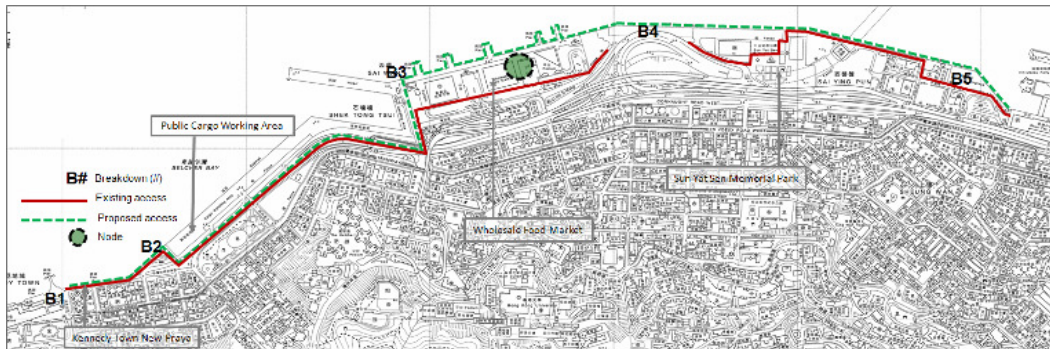


The Six Selected Areas



1. Sai Wan to Sheung Wan 2. CWB Typhoon Shelter to Quarry Bay Park 3. Shau Kei Wan Typhoon Shelter to Heng Fa Chuen 4. Yau Tong to Kowloon Bay 5. To Kwa Wan to Hong Hum 6. Yau Ma Tei Typhoon shelter to Tai Kok Tsui

Area 1 - Sai Wan to Sheung Wan



Area 1 - Sai Wan to Sheung Wan



Existing
Existing site photo

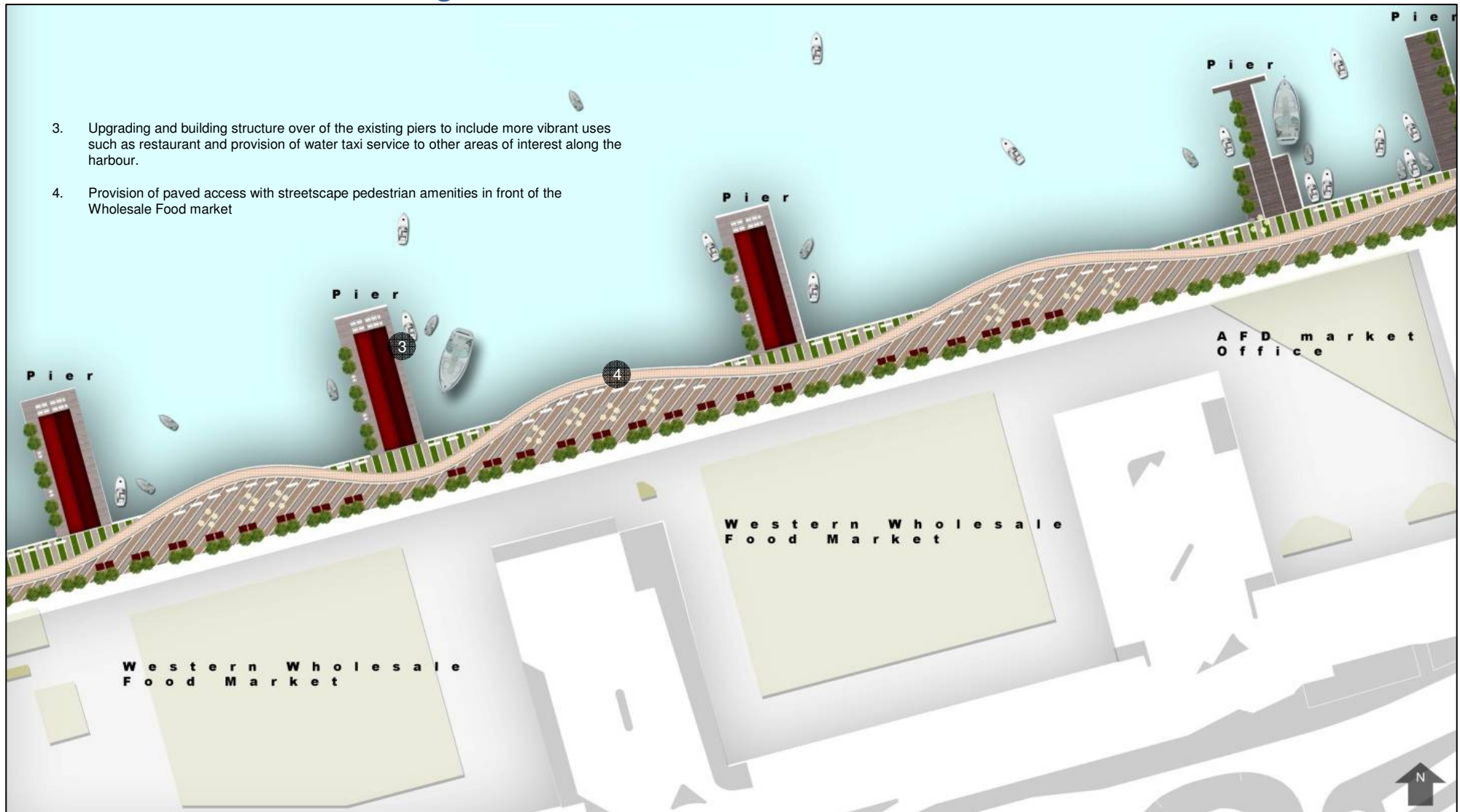


Proposed

Photomontage: To show upgrading and renovation of the existing ferry piers as restaurant and provision of water taxi service to other areas of interest along the harbour.

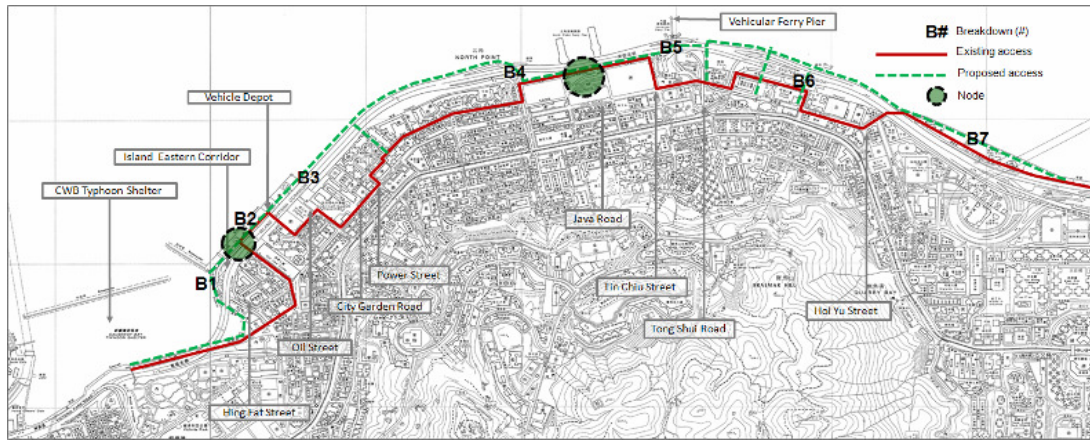
Area 1 - Sai Wan to Sheung Wan

- 3. Upgrading and building structure over of the existing piers to include more vibrant uses such as restaurant and provision of water taxi service to other areas of interest along the harbour.
- 4. Provision of paved access with streetscape pedestrian amenities in front of the Wholesale Food market



Detailed proposal at the Western Wholesale Food Market and the Piers

Area 2. Causeway Bay Typhoon Shelter to Quarry Bay Park

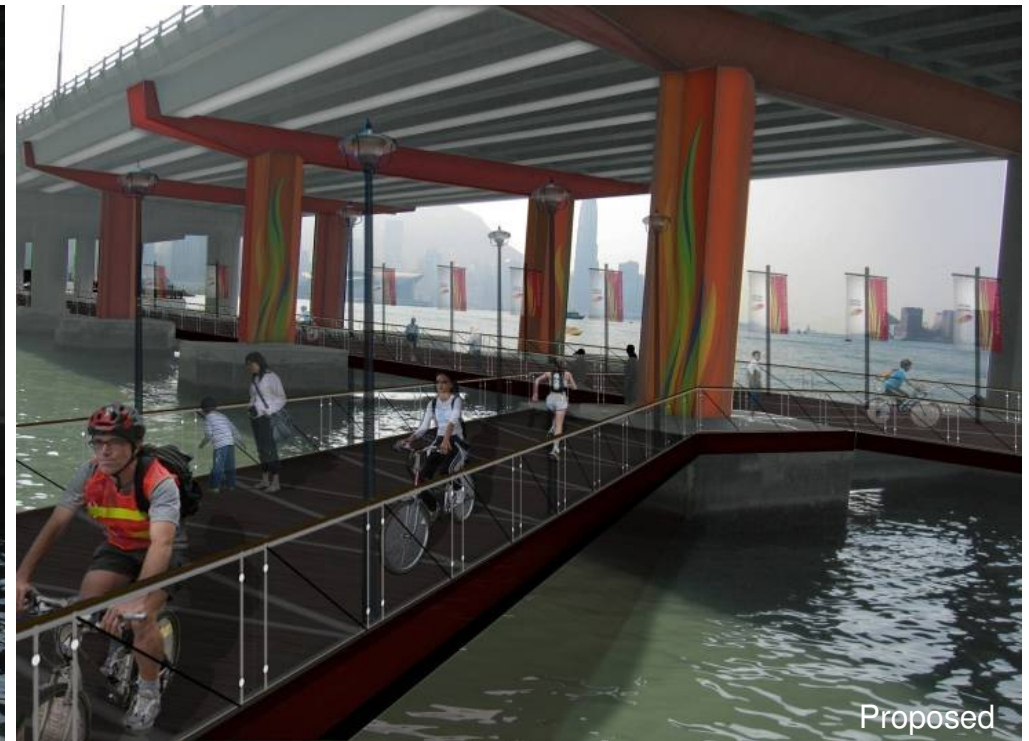


Area 2. Causeway Bay Typhoon Shelter to Quarry Bay Park



Existing

Existing site photo



Proposed

Photomontage : The construction of a cantilever extension to allow passage past the HEC substation site

Area 2. Causeway Bay Typhoon Shelter to Quarry Bay Park



Existing

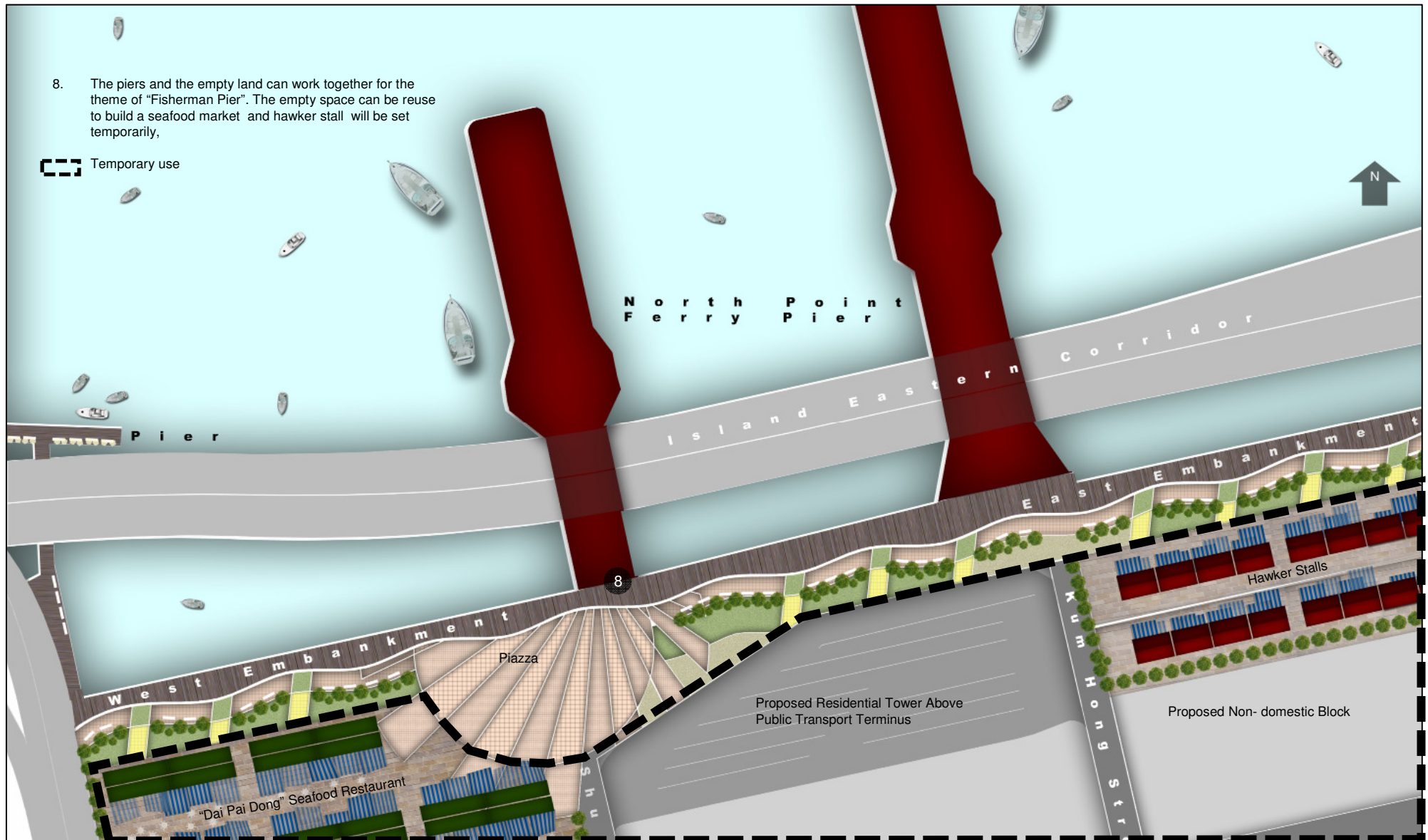
Existing North Point Ferry Pier



Proposed

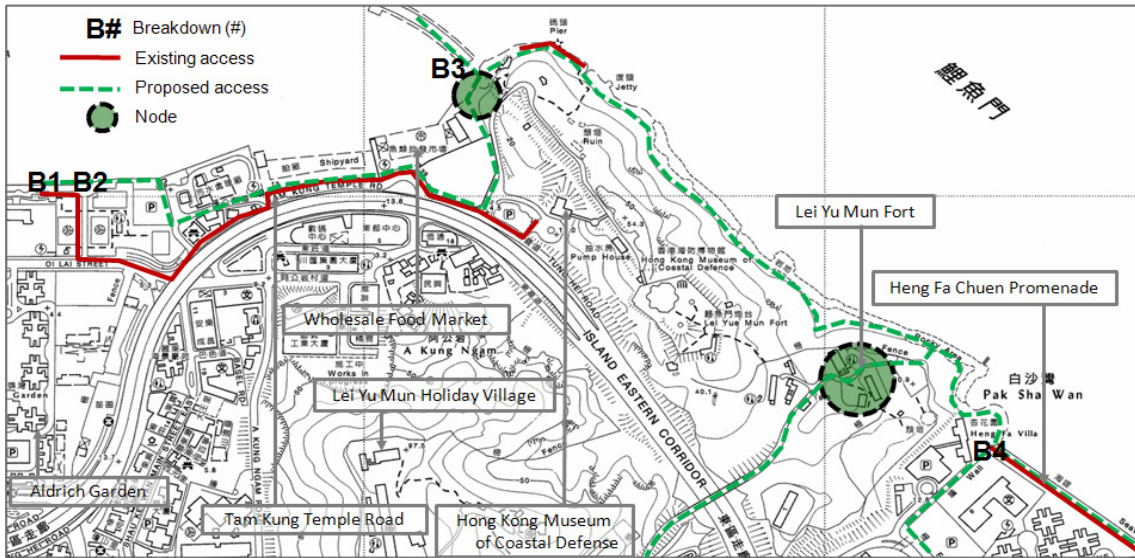
Photomontage :The North Point (West) Ferry Pier can convert to a Seafood restaurant;

Area 2. Causeway Bay Typhoon Shelter to Quarry Bay Park



Detailed proposal at North Point Ferry Pier area

Area 3. Shau Kei Wan Typhoon Shelter to Heng Fa Chuen

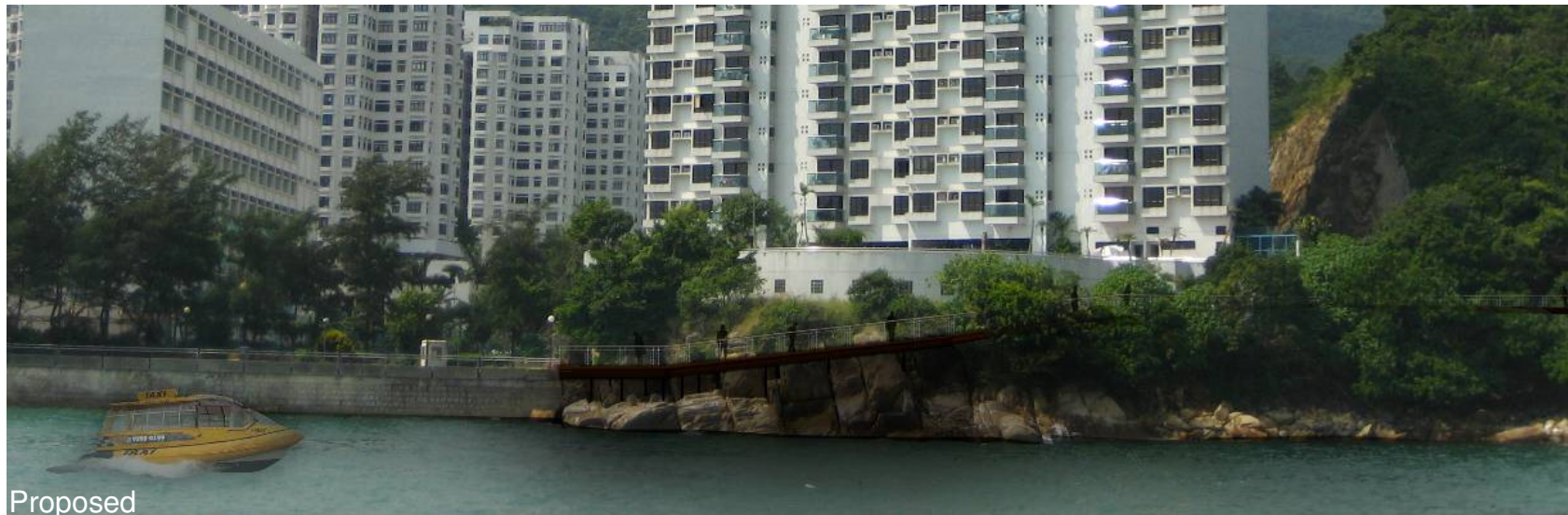


Area 3. Shau Kei Wan Typhoon Shelter to Heng Fa Chuen



Existing

Existing site photo



Proposed

Elevated promenade will be built above the rock

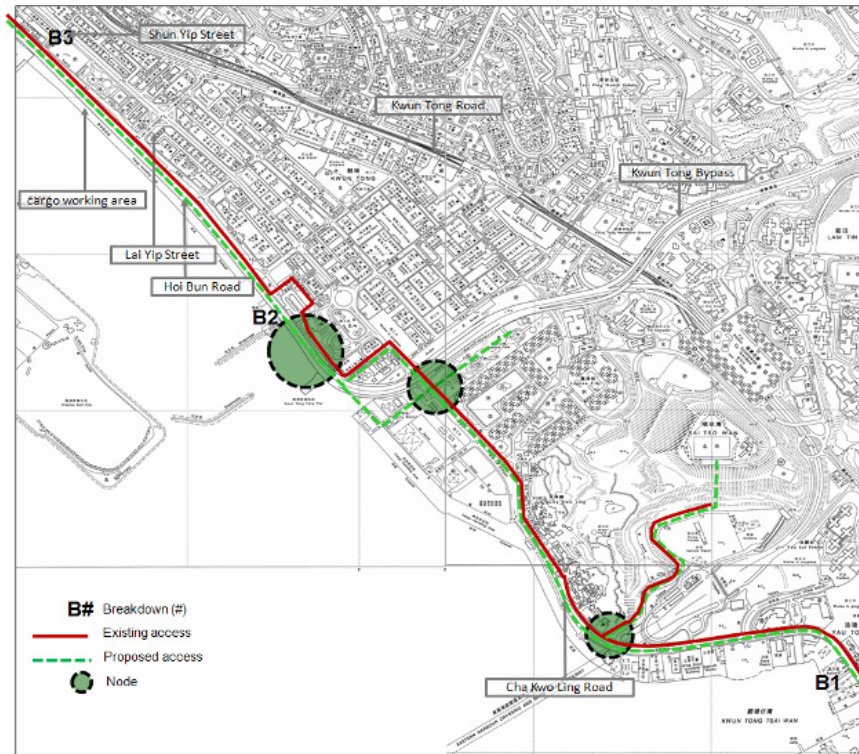
Area 3. Shau Kei Wan Typhoon Shelter to Heng Fa Chuen



- 5. Elevated promenade will be built above the rock
- 6. Restaurant and tea house with outdoor seating to let visitors enjoy the lovely nature environment .
- 7. Water feature
- 8. Hostel provides another alternative type of accommodation .

Detailed proposal connecting the Lei Yu Mun Fort and the promenade

Area 4 - Yau Tong to Kowloon Bay



Area 4 - Yau Tong to Kowloon Bay



Existing

The Kwun Tong Ferry Pier



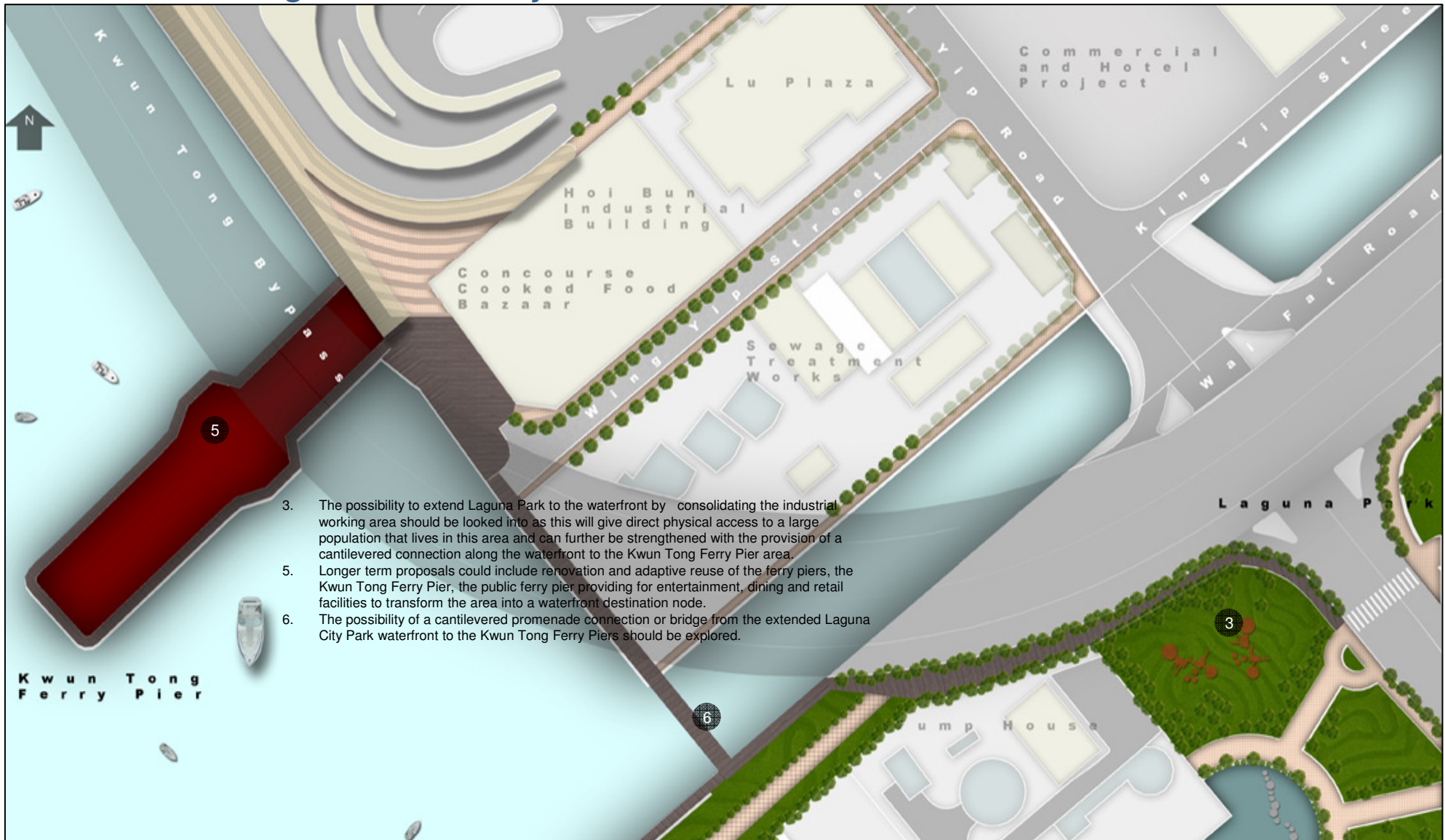
Proposed

Photomontage: Longer term proposals could include renovation and adaptive reuse of the ferry piers, the Kwun Tong Ferry Pier, the public ferry pier providing for entertainment, dining and retail facilities to transform the area into a waterfront destination node.

Area 4 - Yau Tong to Kowloon Bay



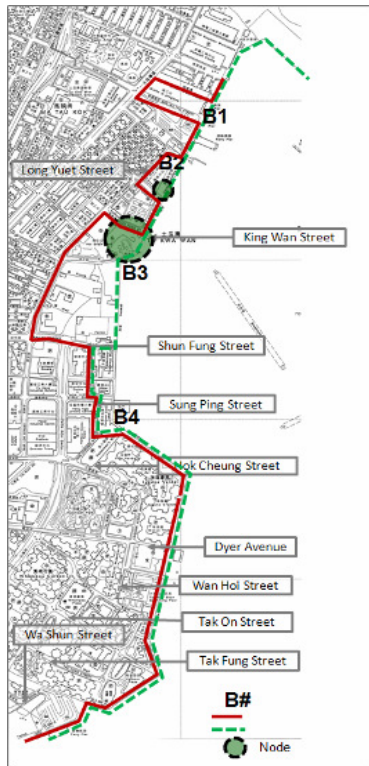
Area 4 - Yau Tong to Kowloon Bay



3. The possibility to extend Laguna Park to the waterfront by consolidating the industrial working area should be looked into as this will give direct physical access to a large population that lives in this area and can further be strengthened with the provision of a cantilevered connection along the waterfront to the Kwun Tong Ferry Pier area.
5. Longer term proposals could include renovation and adaptive reuse of the ferry piers, the Kwun Tong Ferry Pier, the public ferry pier providing for entertainment, dining and retail facilities to transform the area into a waterfront destination node.
6. The possibility of a cantilevered promenade connection or bridge from the extended Laguna City Park waterfront to the Kwun Tong Ferry Piers should be explored.

Proposals for the Laguna Park area

Area 5 - To Kwa Wan to Hung Hom



Area 5 - To Kwa Wan to Hung Hom



Existing

The abandoned two-level vehicular ferry pier.



Proposed

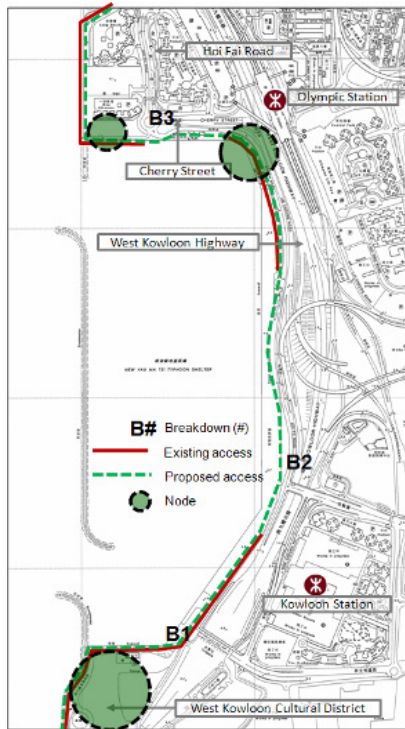
Photomontage: The abandoned double-storey vehicular ferry pier can be renovated to provide a small café or kiosks and eating facilities on the docks. A staircase however would need to be built for pedestrians to get to the second level;

Area 5 - To Kwa Wan to Hung Hom



Detailed proposal along To Kwa Wan waterfront

Area 6. Yau Ma Tei Typhoon shelter to Tai Kok Tsui



Area 6. Yau Ma Tei Typhoon shelter to Tai Kok Tsui



Existing

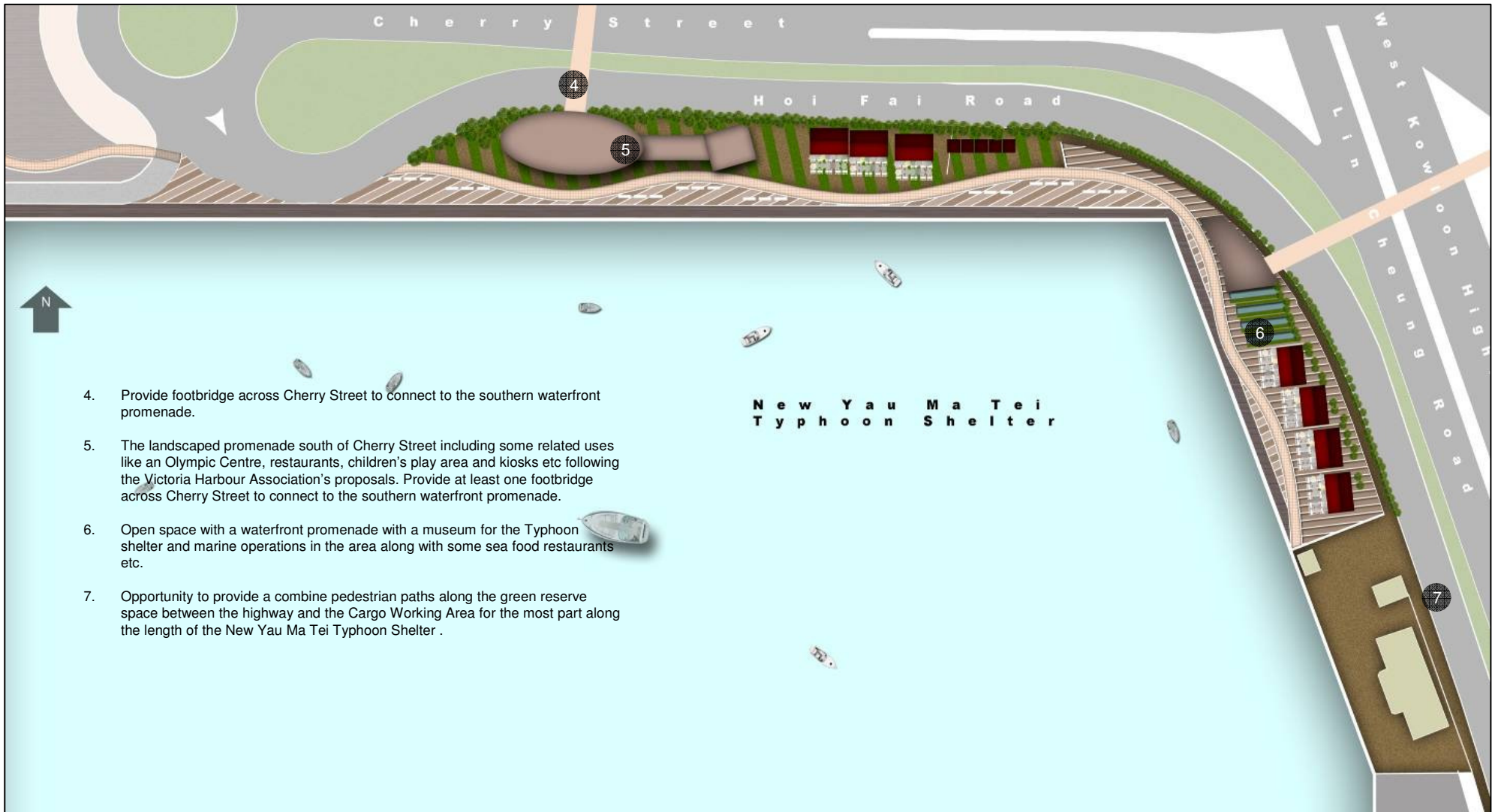
Existing waterfront promenade



Proposed

Photomontage: Open space with a waterfront promenade with a museum for the Typhoon shelter and marine operations in the area along with some sea food restaurants etc.

Area 6. Yau Ma Tei Typhoon shelter to Tai Kok Tsui



4. Provide footbridge across Cherry Street to connect to the southern waterfront promenade.
5. The landscaped promenade south of Cherry Street including some related uses like an Olympic Centre, restaurants, children's play area and kiosks etc following the Victoria Harbour Association's proposals. Provide at least one footbridge across Cherry Street to connect to the southern waterfront promenade.
6. Open space with a waterfront promenade with a museum for the Typhoon shelter and marine operations in the area along with some sea food restaurants etc.
7. Opportunity to provide a combine pedestrian paths along the green reserve space between the highway and the Cargo Working Area for the most part along the length of the New Yau Ma Tei Typhoon Shelter .

Detailed plan of the Cherry Street proposal

Area 6. Yau Ma Tei Typhoon Shelter to Tai Kok Tsui

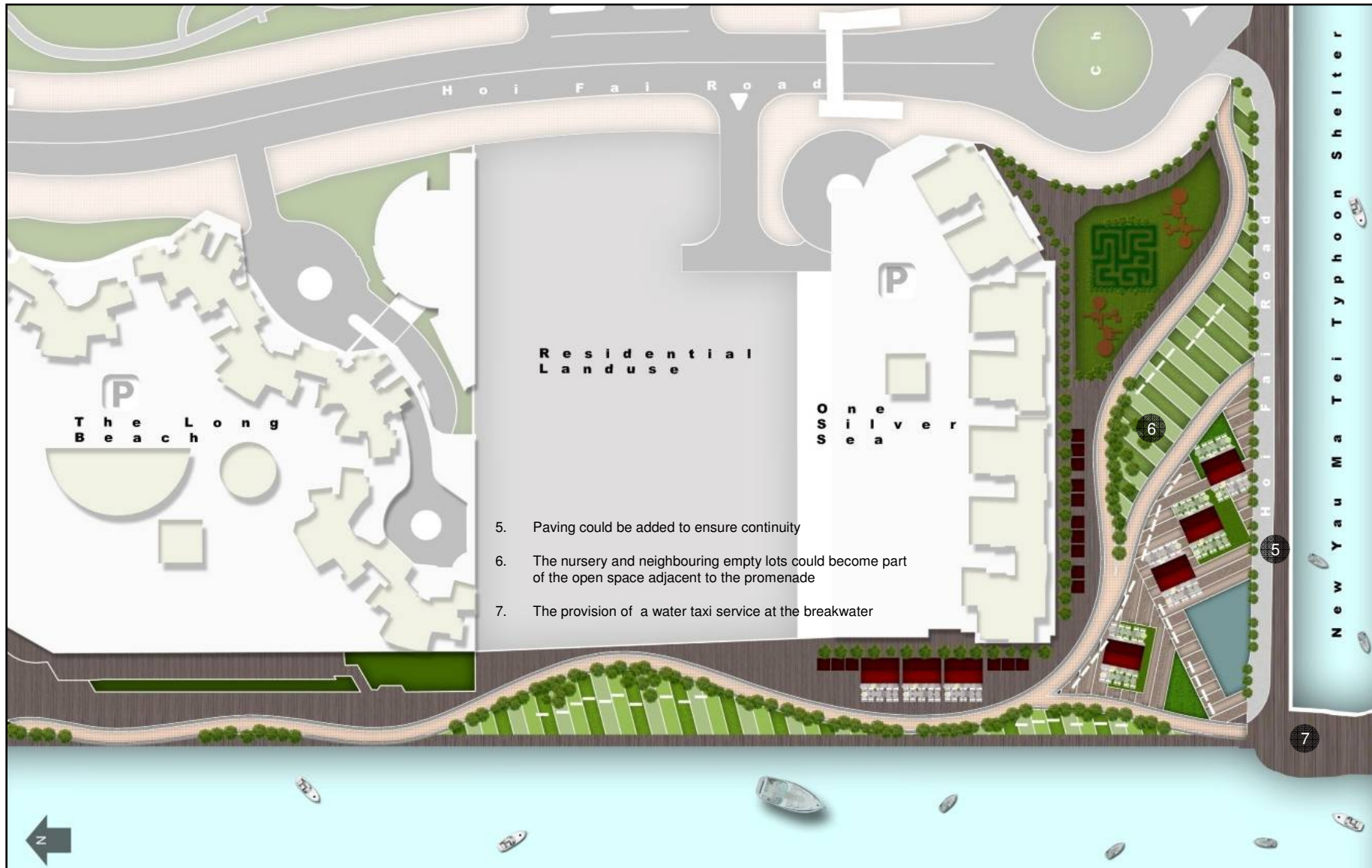


Existing green reserve space between the highway and cargo working area



Photomontage: Opportunity to provide a combine pedestrian paths along the green reserve space between the highway and the Cargo Working Area for the most part along the length of the New Yau Ma Tei Typhoon Shelter .

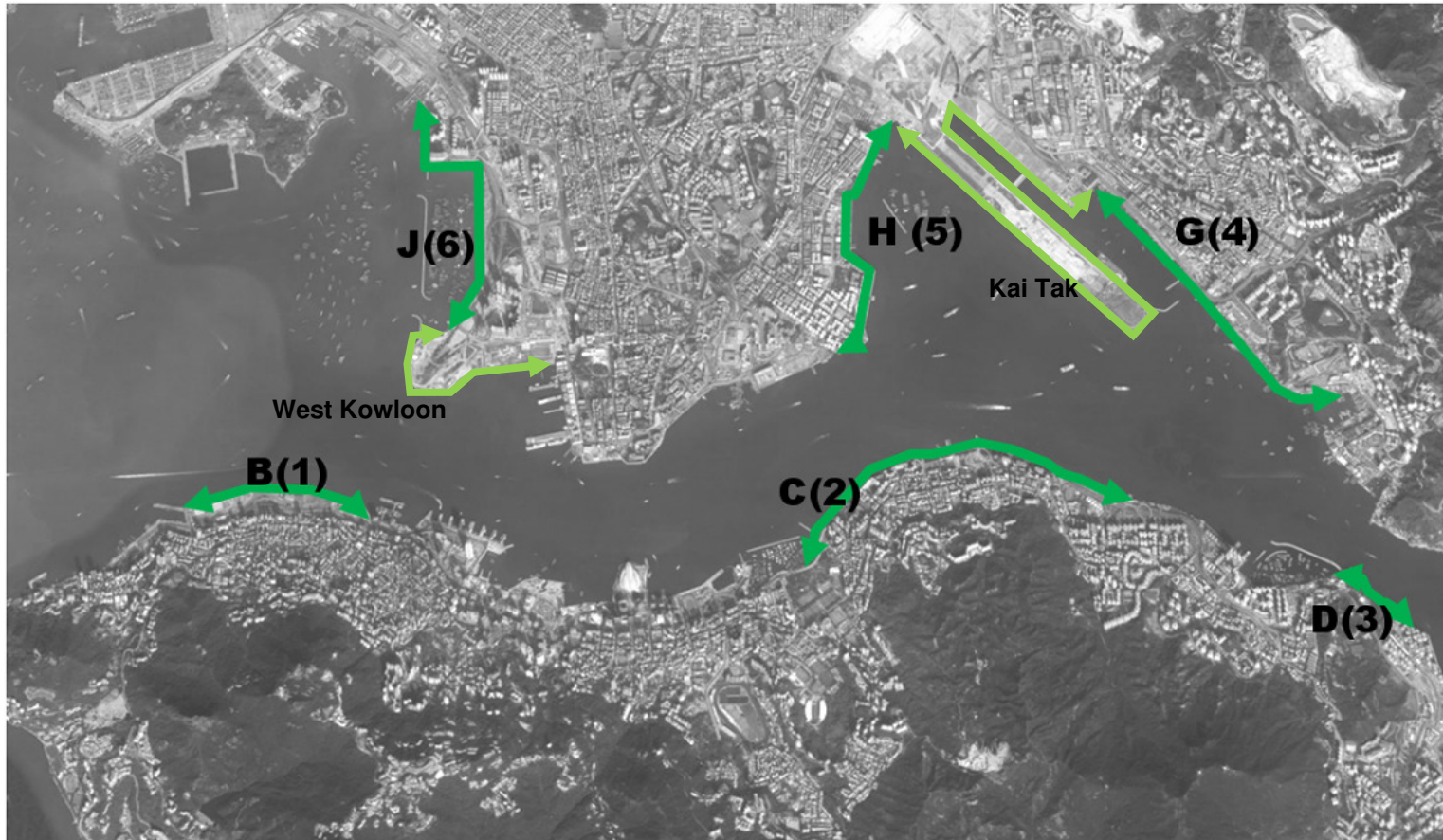
Area 6. Yau Ma Tei Typhoon Shelter to Tai Kok Tsui



- 5. Paving could be added to ensure continuity
- 6. The nursery and neighbouring empty lots could become part of the open space adjacent to the promenade
- 7. The provision of a water taxi service at the breakwater

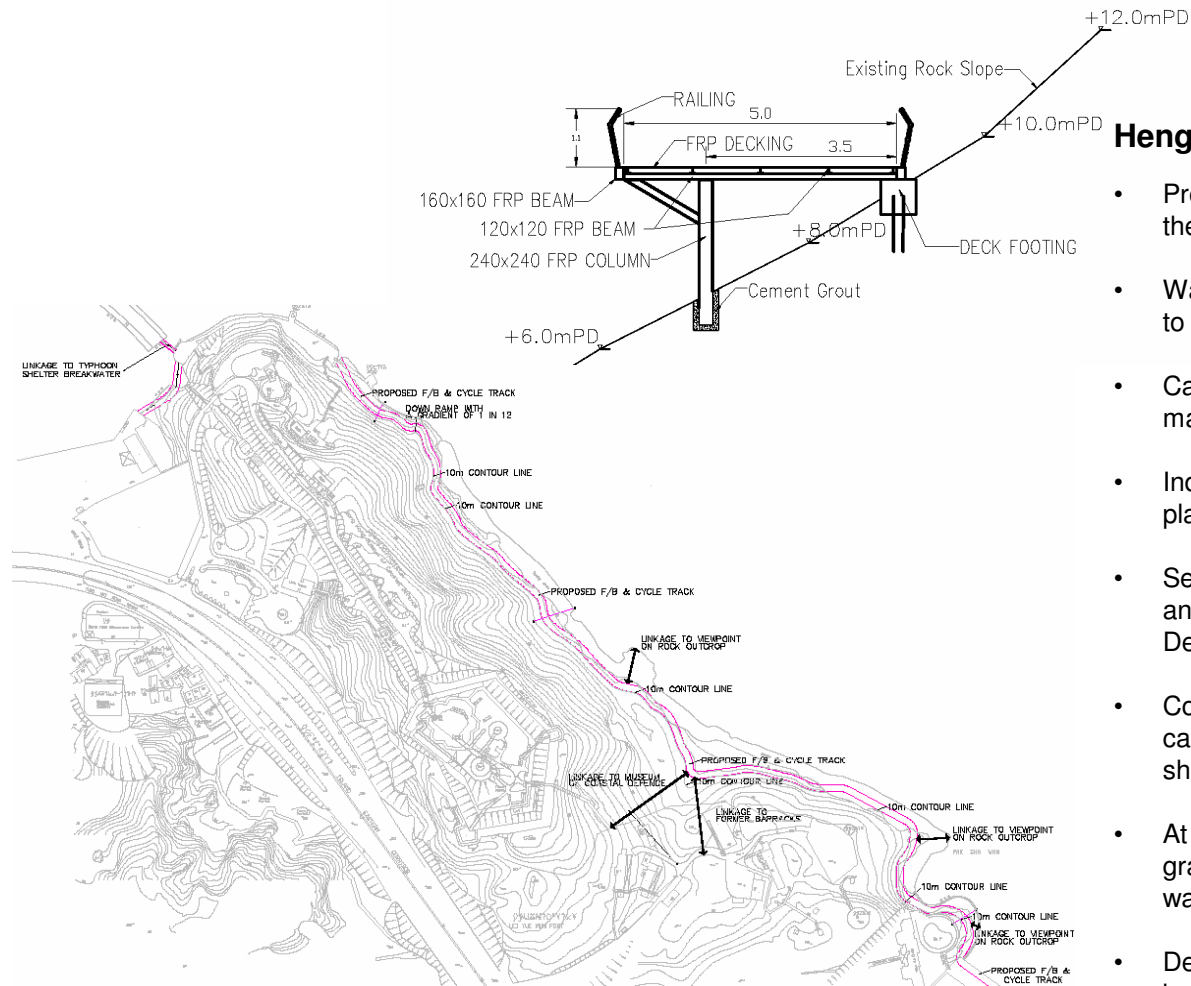
Detailed plan of proposals in the new development area

Hong Kong and Kowloon Harbourfront Paths and Maps



1B Sai Wan to Sheung Wan	422	3609	4031	Site	Temporary access (m)
2C. CWB Typhoon Shelter to Quarry Bay Park	2674	3825	6499	West Kowloon	2746
3D. Shau Kei Wan Typhoon Shelter to Heng Fa Chuen	623	1339	1962	Kai Tak	6212
4G. Yau Tong to Kowloon Bay	1380	1757	3137		8958
5H. Hong Hom to To Kwa Wan	2854	597	3451		
J6. Yau Ma Tei Typhoon shelter to Tai Kok Tsiu	823	1824	2647		
Total (m)	8776	12951	21727		

Engineering Considerations



Heng Fa Chuen Section – Engineering Aspects

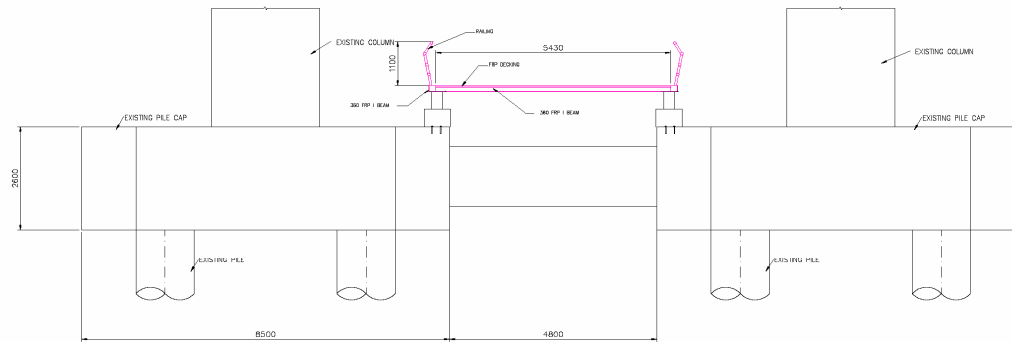
- Propped cantilever deck supported on a concrete pad at the inshore end and a column socketed into the slope
- Walkway is 5m wide and at 10m elevation above sea level, to minimise wave impacts
- Can be constructed of fibre-reinforced plastic to minimise maintenance and save weight
- Individual sections constructed off-site and positioned in place by barge
- Seaward connections to viewpoints along the coastline, and landward connections back to Museum of Coastal Defence and surrounding areas
- Connects to Heng Fa Chuen promenade to the east, via cantilevered section around headland, and to typhoon shelter breakwater via bridge
- At western end cycle track will slope down (at a shallow gradient), avoiding temple, and will connect with existing waterfront promenade at Coastal Defence Museum
- Designed to have minimal impact on existing vegetation, both during construction and in use, and intended to integrate with existing natural landscape.

Engineering Considerations

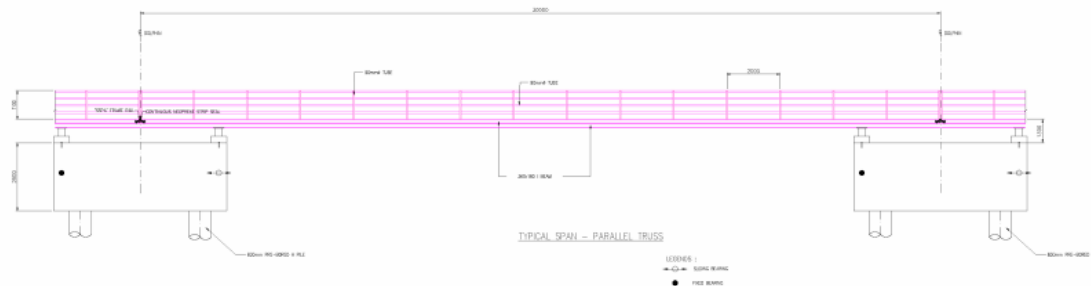
Heng Fa Chuen Section – Photomontage



Engineering Considerations



TYPICAL SECTION BETWEEN EXISTING PILE CAPS (WITH TIE BEAM)



TYPICAL SPAN - PARALLEL TRUSS

Island Eastern Corridor – Engineering Aspects

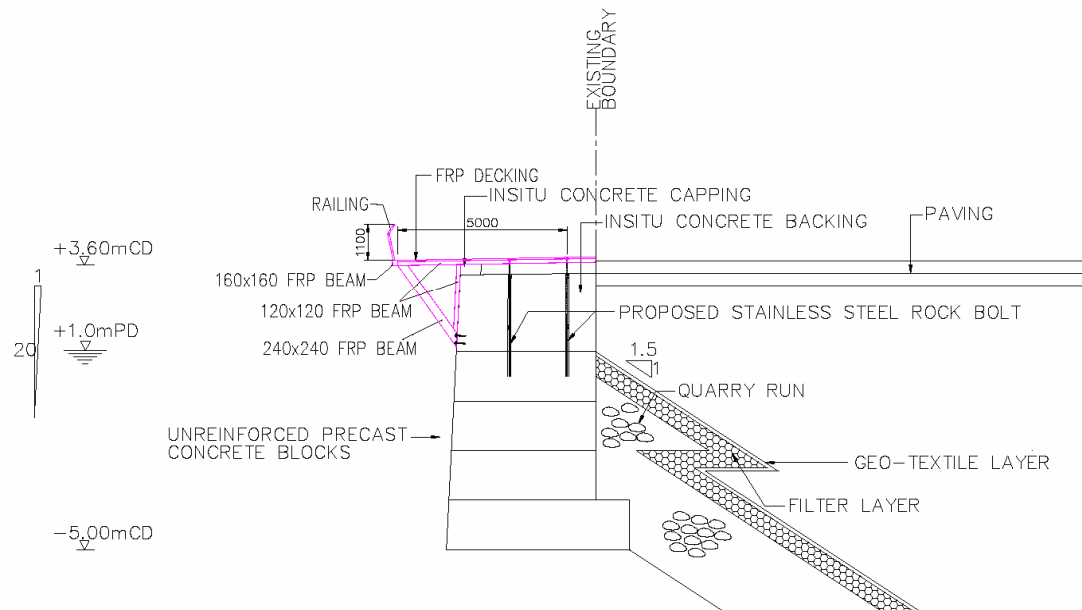
- Continuous series of bridges spanning between the pile caps of the expressway abutments
- To avoid any additional pile foundations, bridge will use spare design capacity in the existing pile caps – expected to be adequate spare design capacity, given the minimal additional loading in comparison with the loading from the existing expressway
- Bridges would take the form of lightweight truss structure, to be constructed either of steel or fibre-reinforced plastic
- To avoid wave loading during typhoons and elevated tide levels, bridges would be raised above the danger level on support plinths.
- To accommodate possible vessel impacts, bridges would be fixed onto rubber shear fenders bolted to the plinths. Plinths would be fixed to the existing piled supports using stainless steel resin anchors.
- Fenders will absorb the impact energy from small vessels, eliminate shock loads and prevent the bridges being torn from their supports.
- Since the bridges lie inside the eastern corridor pile caps, there should be no requirement for protection from vessels other than small leisure craft or very small ferries
- Need to maintain access to existing piers. Walkways will tie into existing piers in most cases. In one case, walkway will need to be elevated to allow small vessels to pass beneath

Engineering Considerations

Island Eastern Corridor Section – Photomontage



Engineering Considerations



Cantilevered Section – Engineering Aspects

- Outline design of typical section to be applied for sections of existing vertical seawall, where no existing access is possible
- Cycle track structure is proposed to be cantilevered over the sea from the wall.
- Extent of cantilever will depend on the width of the available land. Cantilever will be anchored into the seawall using stainless steel rock bolts or grouted stainless steel reinforcement bars.
- In order to reduce the structure weight and maintenance costs FRP structural sections are proposed

Implementation



Lands Department to review all blockages created by Government fencing and remove or alter them to facilitate access along waterfront

Implementation



Engineering Solutions

- Create PWP Project Vote to fund connectivity projects;
- Expand walkways with cantilever structures;
- Create bridges and walkways where public demand is already evident.

Implementation



Establish a clear system of signage which makes the connections clear and legible



Coordinate government land holdings with departments to exploit opportunities that exist but are not seen as connections

Implementation



Government must prioritize appropriate permanent and temporary uses along waterfront:

- Tender sites identified for permanent Waterfront Commercial and Tourism uses;
- Develop all waterfront open space sites as a priority;
- Develop all GIC sites with community input;
- Only permit temporary uses which enhance waterfront ; stalls, kiosks, cafes, retail, recreational uses;
- Require all waterfront STT's to provide a 10 -15 m wide public walkway along waterfront edge;
- Give as long a term as possible to encourage small businesses to invest.

Implementation



We could create an interesting and fun waterfront with appropriate temporary business activities. (Qingdao China)

Thank You !!

Connecting "Forgotten" Waterfronts



Harbour Business Forum
www.harbourbusinessforum.com
May 2008