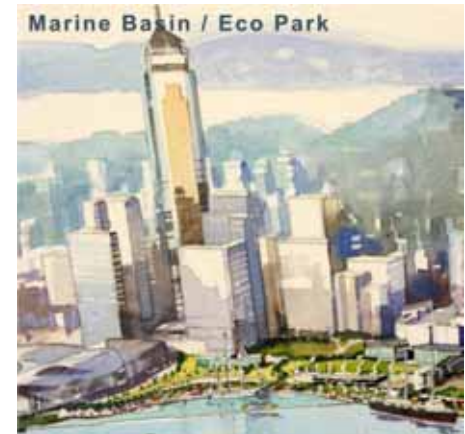




The HK UDA

- The Hong Kong Urban Design Alliance (HK UDA) was established in 2001
- Objective - Improving the quality of urban life in Hong Kong by fostering greater awareness and promoting higher standards of urban design
- An Alliance of:
 - HKIP; and
 - HKIA.
 - With participation from the HKILA, AIA-HK (UDC) and the PIA (HK)
- The UDA promotes a high standard of urban design, the art of making places for people – the public realm – and improving the quality of urban life



Aerial View of Entire Central Waterfront



Previous Central Reclamation Plan



Aerial Photo of Harbour Reclamations



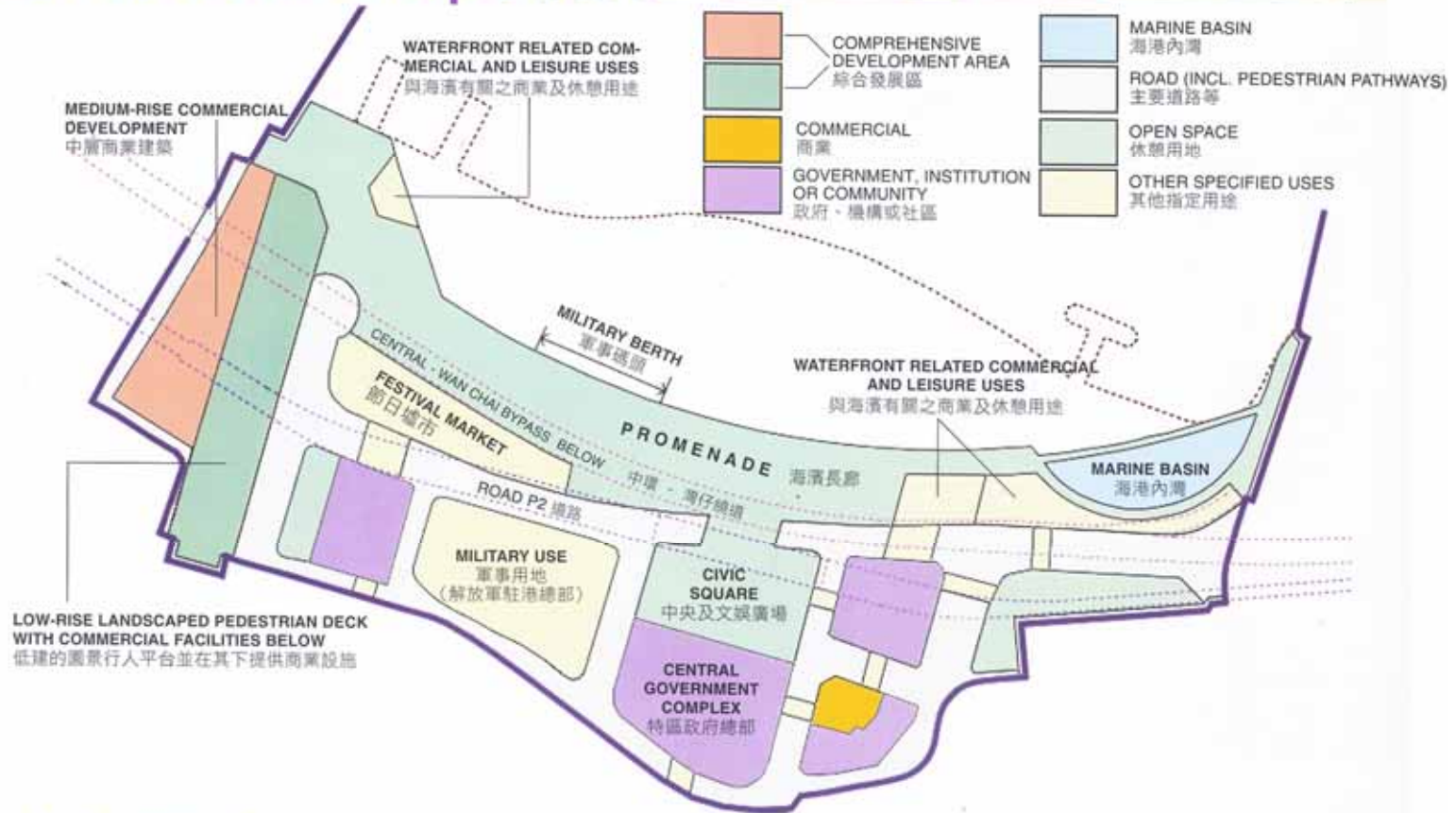
1998 Draft Central Extension OZP

An Outline Zoning Plan has been prepared to translate the design objectives for the New Central Waterfront into a workable arrangement of land uses. The plan provides for an enhanced waterfront, new civic and commercial developments, open space provision, land to reprove uses affected by reclamation and reserves for essential circulation and access.

Schedule of Uses and Areas 土地用途及面積

USES	Ha 公頃	% 百分比	用途
Commercial	0.4	0.8	商業
Comprehensive Development Area	5.2	11.8	綜合發展區
Government, Institution or Community	5.4	12.1	政府、機構或社區
Open Space	13.6	30.9	休憩用地
Other Specified Uses	7.3	16.4	其他指定用途
Major Road, etc.	11.0	25.1	主要道路等
Marine Basin	1.3	2.9	海港內灣
TOTAL PLANNING SCHEME AREA	44.2	100.0	規劃範圍總面積

Land Use Proposals



土地用途建議

經修訂的分區計劃大綱草圖已經擬備，把中區海旁的設計目標轉化為切實可行的土地用途安排，包括闢設優美的海旁、提供新的文娛及商業發展用地及休憩用地，並提供土地，重置受填海工程影響的發展，以及鋪築必要通道。

Aerial View of Harbour with Lines of Potential Reclamation



Government's Current Proposal

- The UDA Submitted specific comments on Government's Consultation Digest
- The UDA Disagrees with many of the claims made by Government in this Document under several headings
- For comparison sake, the same criteria set out in the Government's document has been used for our proposal
- The UDA have commented on these and made a comparison with our own proposal



Designing Hong Kong's Central Waterfront of Hong Kong International Urban Planning & Design Competition



2nd Prize – Hong Kong Waterfront

502



1st Prize – Amphibian Carpet



4th Prize - Sky for Dragon, Earth for People



3rd Prize – Emerald Necklace



Central Harbour - “The Happening

A. Responding to Public Aspirations **Place”**

- “To make Victoria Harbour attractive, vibrant, accessible and symbolic of Hong Kong – a harbour for the people and a harbour of life” (TPB Vision Statement)

- Responsive to principles laid down by the Harbour Enhancement Committee



B. Creating a Vibrant, Green and Accessible Waterfront

- Our proposal offers:

- A mixed use experience;
- Recognizable landmarks;
- A commercially viable harbourfront;
- Humanly scaled pedestrian linkages; and
- Introduction of cultural facilities to create identity and vibrancy

C. A Defined Urban Design Framework

- Our proposal creates a more articulate urban design framework, unifying the development through defined spaces, clear development axes and historical landmarks



Central Harbour - “The Happening Place”

D. Achievement of Diversity and Vibrancy

- Our proposal creates a series of humanly – scaled environments that represent natural extensions to the city that promote the interaction of people and events



E. Development Intensity in Harmony with the Harbourfront

- Our proposal utilizes precisely the same area of commercial gross floor area as the Government scheme, but redistributes this in low-rise buildings which are used to define spaces and pedestrian streets



F. Respecting the Natural Context and Existing Urban Fabric

- Our proposal not only protects the ridgeline, but generates a transition in scale, massing and development intensity towards the harbour, with well defined view corridors, and a ‘green necklace’ of landscape elements which join together both existing and proposed spaces

Central Harbour - “The Happening Place”

G. Ease of Access and Pedestrian Connectivity

- Our proposal entails a pedestrian network almost entirely at grade, with some elevated connections where appropriate, and parkland decks over the road network between the Academy of Performing Arts and the HKCEC.



H. Respecting Cultural Heritage

- Our proposal creates an opportunity to link the traditional with contemporary urban culture by:

- Resurrecting the Queen’s Pier in its original location;
- Proposing the Star Ferry Clock Tower as a monument;
- Proposing an historic tram line; and
- Places for rickshaws, dai pai dongs, street markets or night bazaars

I. Promoting Environmentally Friendly Design and Greening

- Our proposal creates a genuine ‘green’ network of pedestrian avenues, spaces, precincts and promenades, together with a marine basin linked to a proposed maritime museum, a marine walk and a waterfront promenade including an inner harbour...



8 Place Destinations

1. Harbour Place @ Central Ferry Piers
2. Statue Square Esplanade
3. Inner Harbour
4. Central Waterfront Promenade
5. Tamar Green
6. Central Harbour Hotel & Commercial Complex
7. Eco Park & APA Arts Corner
8. Maritime Museum & Marine Basin



8 Place Destinations

1. Harbour Place @ Central Ferry Piers

- Central Harbour Visitor's Centre and Star Ferry Museum near the public piers
- Service Apartments in a low block three storey high over a low podium block
- A second footbridge link from IFC podium and landscaped trellis connection to the ferry piers
- Retail bridge links connecting the low podium block to the ferry piers
- Bus terminus replaced with drop off at ground level under the low podium structure
- Restaurants with alfresco dining within the low podium structure and above ferry piers
- Sunday flea markets under the east west footbridge connecting the ferry piers
- Tram line along the ferry piers connecting to the Esplanade and the Central Boulevard



8 Place Destinations

2. Statue Square Esplanade

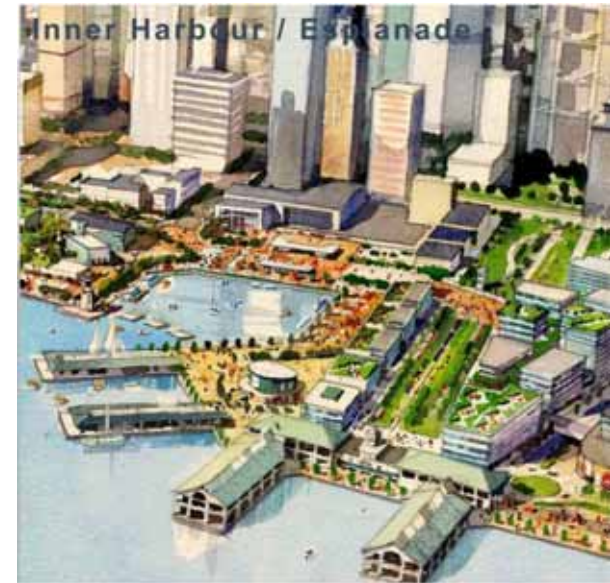
- Tram line along the northern half of the Esplanade from the ferry piers
- Street markets and night markets along the Esplanade
- Chinese New Year Festival parades and Christmas celebrations
- Historic Rickshaw rides along the Esplanade
- Reinstated Star Ferry Clock Tower plaza in it's original location
- Restaurants and alfresco dining including Chinese Restaurants and Dai Pai Dongs
- Public art displays and informal street performances along the Esplanade
- Shops and indoor sports complex and parking below the Esplanade



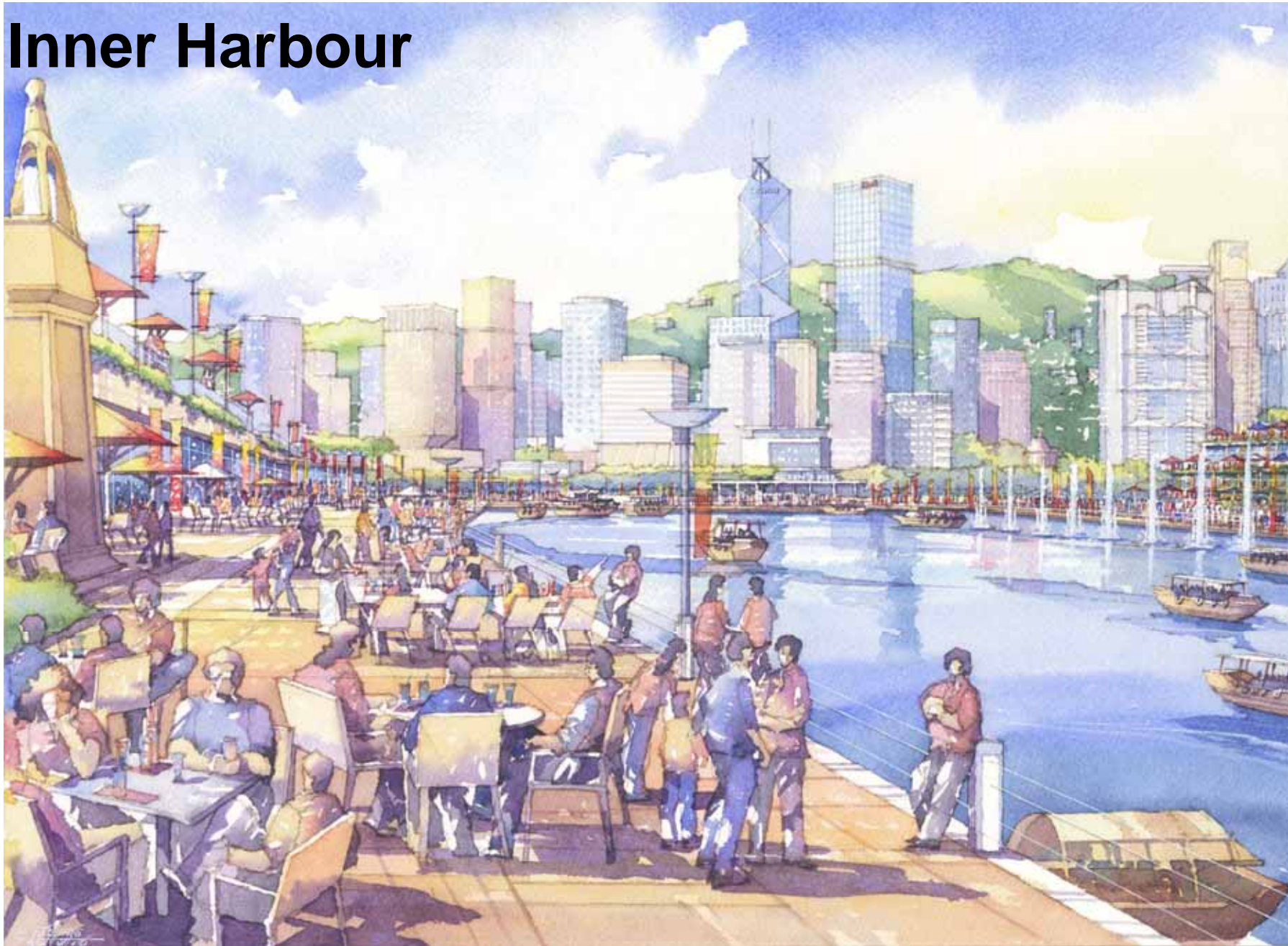
8 Place Destinations

3. Inner Harbour

- Queen's Pier reinstated in its original location with an extension of Edinburgh Place at the inner harbour, maintaining its original axis and regaining its function and relationship with the water
- Kiosks and restaurants with alfresco dining wrapping around the Inner Harbour
- Steps wrapping the Inner Harbour allowing people to interact and be close to the water
- Traditional Chinese performances , informal theatre showcased on floating pontoons
- Musical fountains, fireworks and laser shows showcased within the inner harbour
- Small boat rides within the Inner Harbour from the new Pier at Edinburgh Place extension
- Light house over pumping station at the mouth of the Inner Harbour
- Central Harbour Visitor's Centre and Star Ferry Museum near the public piers



Inner Harbour



8 Place Destinations

4. Central Waterfront Promenade

- Restaurants and alfresco dining along waterfront promenade and courtyards
- Cycling, walking, roller blading along a waterfront promenade
- Arboretum Nature walk along a meandering alignment
- Historic tram line along Central Boulevard (Road P2)
- Landscaped gardens and fountains in PLA Square
- PLA Pier Temporary Urban Beach – Central Plage
- PLA Pier LIDO – Public Swimming Pool
- Central Boulevard with mature trees along wide pavements on either side of the Road P2



8 Place Destinations

5. Tamar Green

- City Vision Urban Centre & Citizen Square
- Infrastructure Exhibition below Citizen Square
- Arboretum Nature walk along a meandering alignment
- Bauhinia Plaza with the relocated Golden Bauhinia in front of Tamar
- Tamar Green, rolling landscaped open space for flexible use
- Restaurants and alfresco dining along the waterfront promenade
- Children's playground and carousel near Citizen Square
- Festival and Seasonal Parades



8 Place Destinations

6. Central Harbour Hotel & Commercial Complex

- Hotel and commercial complex
- Service Apartment & club House
- Floating Hotel and harbour cruises
- Restaurants and alfresco dining
- Shops and retails uses along the waterfront promenade
- Walking, cycling and jogging along the waterfront promenade
- Arboretum Nature walk and sculpture
- Indoor sports complex and parking underground



8 Place Destinations

7. Landscaped Eco Park & APA Arts Corner

- APA Plaza for outdoor performances & events
- APA Extension and Visual Arts Centre
- Landscaped Eco Park over extended deck linkage
- Landscaped trellis connection between HKCEC & Eco Park
- Public Art & Community Exhibitions
- Shops, restaurants and alfresco dining
- HKCEC Truck marshalling area below APA Plaza



8 Place Destinations

8. Maritime Museum & Marine Basin

- Moored sail boats and tall ships
- Shops and Marina Walk
- Marine Centre and training facility
- Maritime Museum
- Pedestrian Causeway linking HKCEC to the Floating hotel
- Restaurants and alfresco dining
- Water taxi pick up point



Maritime Museum & Marine Basin



Central Harbour – “The Happening Place” Model



Central Harbour – “The Happening Place” Model



Saleable Commercial Sites



ACHIEVEMENT OF STATED CRITERIA

CRITERIA	GOVERNMENT PROPOSALS	UDA PROPOSALS
1. Responding to Public Aspirations	?	✓
2. Creation of a Vibrant, Green and Accessible Waterfront	x	✓
3. A Refined Urban Design Framework	x	✓
4. Achievement of Diversity and Vibrancy	x	✓
5. Development Intensity in Harmony with the Harbourfront	x	✓
6. Respecting the Natural Contact and Existing Urban Fabric	✓	✓
7. Ease of Access and Pedestrian Connectivity	✓	✓
8. Respect for Cultural Heritage	?	✓
9. Promotion of Environmental Friendly Design and Greening	✓	✓

12 Key Changes

1. Same built GFA but redistributed;
2. More public open space strategically distributed;
3. Human scaled inner harbour for recreational and boating uses;
4. Dedicate less land for roads and interchanges;
5. Removal of tall blocks between the IFC and the Central Ferry Piers and add a storey of development above the existing piers;
6. Introduce a pedestrian esplanade between Statue Square and the Star Ferry, while keeping the old Star Ferry Clock tower located in its original position;



12 Key Changes

7. Reinstate Queen's Pier in its original axis to reflect its original function and prominence;
8. Relocate the PLA Berthing arrangement to a pier structure;
9. The incorporation of a historic tram line from Pier 1 to the HKCEC.
10. Introduce the new Maritime Museum;
11. Contrasting spaces, plazas, and precincts, defined by low-rise buildings along and adjoining the waterfront promenade, including a festival market place; and
12. Predominantly ground level pedestrian circulation system, including at grade pedestrian crossings across Central Boulevard (Road P2) providing direct linkages with Central core..



ESTABLISHED HARBOUR PLANNING PRINCIPLES

The Harbour Planning Principles were developed and are monitored by the Harbour-front Enhancement Committee as a set of guidelines for all individuals and organisations to facilitate the sustainable planning, preservation, development and

Preserving Victoria Harbour management of Victoria Harbour and the harbour-front areas.

Principle 1 : Victoria Harbour must be protected and preserved for Hong Kong people and visitors as a special public asset, a natural and cultural heritage asset, and a driver for the creation of economic and social values.

Stakeholder Engagement

Principle 2 : All sectors of the community must be engaged at an early stage and on an on-going basis in the planning, development and management of Victoria Harbour and its harbour-front areas through transparent and inclusive consensus building processes.

Sustainable Development

Principle 3 : The planning, development and management of Victoria harbour and its harbour-front areas should embrace the principles of sustainable development to balance and cater for the economic, social and environmental needs of all sectors of the present generation, without compromising the needs of future generations.

Integrated Planning

Principle 4 : Integrated and long-term planning, development and management of infrastructure, land and marine uses, and water quality is essential to ensure that Victoria Harbour and its harbour-front areas support and enhance the economic, environmental and social aspirations of Hong Kong.



ESTABLISHED HARBOUR PLANNING PRINCIPLES

The Harbour Planning Principles were developed and are monitored by the Harbour-front Enhancement Committee as a set of guidelines for all individuals and organisations to facilitate the sustainable planning, preservation, development and management of Victoria Harbour and the harbour-front areas.

Proactive Harbour Enhancement

Principle 5: The planning, development and management of Victoria Harbour must proactively enhance the harbour and its harbour-front areas as Hong Kong's symbol of urban design excellence and Hong Kong's brand identity to the international community.

Vibrant Harbour

Principle 6: It is essential to balance the use of the harbour to provide both a maritime and logistics hub for the safe and efficient passage of people and goods, and as a cultural and leisure facility. Both marine and land-side activities must cater to and balance with the aspirations of all sectors of the community.

Accessible Harbour

Principle 7: Victoria Harbour must integrate with the hinterland in a comprehensive manner, including ample unrestricted and convenient visual and physical access for pedestrians, preferably at grade, to and along the Harbour as well as the harbour-front areas.

Public Enjoyment

Principle 8: The planning, development and management of Victoria Harbour and its harbour-front areas should maximize opportunities for public enjoyment. Land required for and the impact from infrastructure developments, utility installations and land use incompatible with the harbour planning principles should be minimized.





A Access

Diverse and Accessible Waterfront

Create a diverse and accessible pedestrianised waterfront that enhances Hong Kong people's quality of life.

A1 Enable Access

Prioritize waterfront access to pedestrians via clear corridors and land and marine-based public transportation.

A2 Open to all Users

Make the harbour usable for all people by creating a variety of activities that provide a sense of enjoyment, a sense of place.

A3 Connect Promenade

Create memorable experiences along a continuous waterfront promenade that invites the public back for more.



B Business

Business and Economic Opportunities

Create a business and economic framework that captures the value of the harbour's global competitive advantage.

B1 Leverage Waterfront

Promote business and commercial uses that benefit from direct access to the harbour and uniquely contribute to harbour activity.

B2 Build Harbour Brand

Protect, increase and capitalise upon the real estate value, prestige of address, and brand identity imparted by the waterfront and skyline.

B3 Diversify & Synergize

Encourage synergies that come from diverse economic activity across the whole harbour.



E Environment

Enhance Hong Kong's Key Environmental Asset

Enhance and preserve the harbour's cultural and environmental heritage.

E1 Celebrate Heritage

Enrich the many parts of the waterfront by reflecting the heritage and culture of its shoreline and interior neighbourhoods.

E2 Reduce Infrastructure

Enhance the harbour's natural environment by reducing the dominance of roadways, logistic, industrial and utilitarian activities at the waterfront.

E3 Improve Breatheability

Enable circulation of air and waterfront senses to and from the harbour and the city.



Thank You

